

STRUCTURES INSPECTION FIELD REPORT

2-DIST
01

B.I.N.
0FJ

ROUTINE INSPECTION

BR. DEPT. NO.
A-13-025

CITY/TOWN ASHFIELD	8-STRUCTURE NO. A13025-0FJ-MUN-NBI	11-Kilo. POINT 000.000	41-STATUS A:OPEN	90-ROUTINE INSP. DATE MAR 31, 2025
07-FACILITY CARRIED HWY BURTON HLL R	MEMORIAL NAME/LOCAL NAME Br No 5 Burton	27-YR BUILT 1939	106-YR REBUILT 0000	YR REHAB'D (NON 106) 0000
06-FEATURES INTERSECTED WATER SOUTH RIVER	26-FUNCTIONAL CLASS Rural Local	DIST. BRIDGE INSPECTION ENGINEER M. P.E. McCabe <i>Michael P. McCabe</i>		
43-STRUCTURE TYPE 302 : Steel Stringer/Girder	22-OWNER Town Agency	21-MAINTAINER Town Agency	TEAM LEADER K. Trunfio <i>K. Trunfio</i>	PROJ MGR Michael Baker Intl Inc
107-DECK TYPE 1 : Concrete Cast-in-Place	WEATHER P. Cloudy	TEMP. (air) 16°C	TEAM MEMBERS M. MAZZU	

ITEM 58 7

DECK DEF

1. Wearing Surface	7	-
2. Deck Condition	7	M-P
3. Stay in Place Forms	N	-
4. Curbs	7	M-P
5. Median	N	-
6. Sidewalks	N	-
7. Parapets	N	-
8. Railing	4	S-A
9. Anti Missile Fence	N	-
10. Drainage System	N	-
11. Lighting Standards	N	-
12. Utilities	N	-
13. Deck Joints	N	-
14.	N	-
15.	N	-
16.	N	-

CURB REVEAL (In millimeters)

N	S
80	110

ITEM 59 6

SUPERSTRUCTURE DEF

1. Stringers	N	-
2. Floorbeams	N	-
3. Floor System Bracing	N	-
4. Girders or Beams	6	M-P
5. Trusses - General	N	-
a. Upper Chords	N	-
b. Lower Chords	N	-
c. Web Members	N	-
d. Lateral Bracing	N	-
e. Sway Bracings	N	-
f. Portals	N	-
g. End Posts	N	-
6. Pin & Hangers	N	-
7. Conn Plt's, Gussets & Angles	N	-
8. Cover Plates	N	-
9. Bearing Devices	H	-
10. Diaphragms/Cross Frames	7	-
11. Rivets & Bolts	7	-
12. Welds	N	-
13. Member Alignment	7	-
14. Paint / Coating	5	M-P
15.	N	-

Year Painted X

COLLISION DAMAGE: *Please explain*
None (X) Minor () Moderate () Severe ()

LOAD DEFLECTION: *Please explain*
None (X) Minor () Moderate () Severe ()

LOAD VIBRATION: *Please explain*
None (X) Minor () Moderate () Severe ()

Any Fracture Critical Member: (Y/N) N

Any Cracks: (Y/N) N

ITEM 60 7

SUBSTRUCTURE DEF

1. Abutments	Dive	Cur	7	
a. Pedestals	N	6		-
b. Bridge Seats	N	6		M-P
c. Backwalls	N	7		-
d. Breastwalls	N	7		M-P
e. Wingwalls	N	6		M-P
f. Slope Paving/Rip-Rap	N	N		-
g. Pointing	N	N		-
h. Footings	N	H		-
i. Piles	N	N		-
j. Scour	N	6		M-P
k. Settlement	N	7		-
l.	N	N		-
m.	N	N		-
2. Piers or Bents			N	
a. Pedestals	N	N		-
b. Caps	N	N		-
c. Columns	N	N		-
d. Stems/Webs/Pierwalls	N	N		-
e. Pointing	N	N		-
f. Footing	N	N		-
g. Piles	N	N		-
h. Scour	N	N		-
i. Settlement	N	N		-
j.	N	N		-
k.	N	N		-
3. Pile Bents			N	
a. Pile Caps	N	N		-
b. Piles	N	N		-
c. Diagonal Bracing	N	N		-
d. Horizontal Bracing	N	N		-
e. Fasteners	N	N		-

UNDERMINING (Y/N) If YES please explain N

COLLISION DAMAGE:
None (X) Minor () Moderate () Severe ()

SCOUR: *Please explain*
None () Minor (X) Moderate () Severe ()

I-60 (Dive Report): N I-60 (This Report): 7

93B-U/W (DIVE) Insp 00/00/0000

X=UNKNOWN N=NOT APPLICABLE H=HIDDEN/INACCESSIBLE R=REMOVED

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ITEM 61 6

CHANNEL & CHANNEL PROTECTION

	Dive	Cur	DEF
1.Channel Scour	N	6	M-P
2.Embankment Erosion	N	6	M-P
3.Debris	N	7	-
4.Vegetation	N	7	-
5.Utilities	N	N	-
6.Rip-Rap/Slope Protection	N	N	-
7.Aggradation	N	7	-
8.Fender System	N	N	-

STREAM FLOW VELOCITY:
Tidal () High () Moderate () Low (X) None ()

ITEM 61 (Dive Report): N ITEM 61 (This Report): 6

93b-U/W INSP. DATE:

ITEM 36 TRAFFIC SAFETY

	36	COND	DEF
A. Bridge Railing	0	4	S-A
B. Transitions	0	0	S-A
C. Approach Guardrail	0	0	S-A
D. Approach Guardrail Ends	0	0	S-A

WEIGHT POSTING Not Applicable X

	H	3	3S2	Single
Actual Posting	N	N	N	N
Recommended Posting	N	N	N	N

Waived Date: EJDMT Date:

At bridge		Other Advance	
E	W	E	W
/	/	/	/

CLEARANCE POSTING

	N		S		meter
	ft	in	ft	in	
Actual Field Measurement		0		0	
Posted Clearance		0		0	

At bridge		Advance	
N	S	N	S
/	/	/	/

Signs In Place (Y=Yes, N=No, NR=Not Required)
Legibility/Visibility

ACCESSIBILITY (Y/N/P)

	Needed	Used
Lift Bucket	N	N
Ladder	Y	Y
Boat	N	N
Waders	Y	Y
Inspector 50	N	N
Rigging	N	N
Staging	N	N
Traffic Control	N	N
RR Flagger	N	N
Police	N	N
Other:		
	N	N

TOTAL HOURS 71

PLANS (Y/N): Y

(V.C.R.) (Y/N): N

TAPE#: _____

List of field tests performed:
Hands on, Visual, D-meter

RATING

Rating Report (Y/N): Y

Date:

Inspection data at time of existing rating
I 58: 7 I 59: 6 I 60: 7 Date : 03/02/2015

Recommend for Rating or Rerating (Y/N): Y

If YES please give priority:
HIGH () MEDIUM (X) LOW ()

REASON: Additional asphalt dead load

CONDITION RATING GUIDE (For Items 58, 59, 60 and 61)

CODE	CONDITION	DEFECTS
N	NOT APPLICABLE	
G 9	EXCELLENT	Excellent condition.
G 8	VERY GOOD	No problem noted.
G 7	GOOD	Some minor problems.
F 6	SATISFACTORY	Structural elements show some minor deterioration.
F 5	FAIR	All primary structural elements are sound but may have minor section loss, cracking, spalling or scour.
P 4	POOR	Advanced section loss, deterioration, spalling or scour.
P 3	SERIOUS	Loss of section, deterioration, spalling or scour have seriously affected primary structural components. Local failures are possible. Fatigue cracks in steel or shear cracks in concrete may be present.
C 2	CRITICAL	Advanced deterioration of primary structural elements. Fatigue cracks in steel or shear cracks in concrete may be present or scour may have removed substructure support. Unless closely monitored it may be necessary to close the bridge until corrective action is taken.
C 1	"IMMINENT" FAILURE	Major deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Bridge is closed to traffic but corrective action may put it back in light service.
0	FAILED	Out of service - beyond corrective action.

DEFICIENCY REPORTING GUIDE

DEFICIENCY: A defect in a structure that requires corrective action.

CATEGORIES OF DEFICIENCIES:

M= Minor Deficiency Deficiencies which are minor in nature, generally do not impact the structural integrity of the bridge and could easily be repaired. Examples include but are not limited to: Spalled concrete, Minor pot holes, Minor corrosion of steel, Minor scouring, Clogged drainage, etc.

S= Severe/Major Deficiency Deficiencies which are more extensive in nature and need more planning and effort to repair. Examples include but are not limited to: Moderate to major deterioration in concrete, Exposed and corroded rebars, Considerable settlement, Considerable scouring or undermining, Moderate to extensive corrosion to structural steel with measurable loss of section, etc.

C-S= Critical Structural Deficiency A deficiency in a structural element of a bridge that poses an extreme unsafe condition due to the failure or imminent failure of the element which will affect the structural integrity of the bridge.

C-H= Critical Hazard Deficiency A deficiency in a component or element of a bridge that poses an extreme hazard or unsafe condition to the public, but does not impair the structural integrity of the bridge. Examples include but are not limited to: Loose concrete hanging down over traffic or pedestrians, A hole in a sidewalk that may cause injuries to pedestrians, Missing section of bridge railing, etc.

URGENCY OF REPAIR:

I = Immediate- [Inspector(s) immediately contact District Bridge Inspection Engineer (DBIE) to report the Deficiency and to receive further instruction from him/her].

A = ASAP- [Action/Repair should be initiated by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) upon receipt of the Inspection Report].

P = Prioritize- [Shall be prioritized by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) and repairs made when funds and/or manpower is available].

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REMARKS

BRIDGE ORIENTATION

BIN 0FJ is oriented from west to east and the elevations are north and south. South River flows from north to south. The orientation is consistent with the plans, previous inspection report, and 2015 Rating report.

GENERAL REMARKS

Structure A-13-025 (0FJ) carries Burton Hill Road over South River in the Town of Ashfield (**Sketch 1**). The bridge is a simple single-span structure comprised of five (5) steel beams supporting a reinforced concrete deck overlaid with an asphalt wearing surface. The underside of deck between the beams is designated as Bay 1 through 4 from north to south, with the overhangs designated as the North and South Overhang. The beams are designated as Beam 1 through 5, from north to south. The diaphragms are designated as Diaphragm 1 through 2, from west to east. The substructure consists of two (2) reinforced concrete abutments labeled West Abutment and East Abutment (**Sketch 2 and Photos 1-8**).

WORK ACCESS

The underside of the deck, superstructure and substructure elements were inspected during daytime hours on foot. Wafers and a 16' ladder were utilized to inspect the underside.

CHANNEL PROFILES

Channel profile measurements were taken on the upstream and downstream fascia of the bridge from the top of the rail base to the ground (**Sketch 3**).

ITEM 58 - DECK

Item 58.2 - Deck Condition

The deck has scattered transverse hairline cracks, with and without efflorescence, typically near the East Abutment. There are isolated spalls and honeycomb, with and without exposed rebar. There are scattered spalls with exposed rebar in the deck overhangs. See the following for specific locations and conditions.

- North Overhang, near West Abutment: 1'-0" long x 6" high x 2" deep spall with exposed rebar (no section loss).
- Bay 1, near East Abutment: 10" wide x 6" long x up to 2-1/4" deep spall with exposed rebar (no section loss).
- Bay 2, near East Abutment: 2'-0" wide x 3'-0" long honeycomb.
- Bay 3, near East Abutment: 6-1/2" wide x 1'-0" long x 2-1/2" deep honeycomb with exposed rebar (no section loss) (**Photo 9**).
- Bay 4, between Diaphragms 1 and 2: 4" diameter patch.
- South Overhang: Six (6) up to 8" wide x 4" long x 1" deep spalls with exposed rebar (no section loss).

Item 58.4 - Curbs

The curbs have isolated scale up to 7" long x 3" wide x 2" deep.

Item 58.8 - Railing

The railings typically have scattered areas of peeling paint with light rust throughout. There are scattered tipped posts and up to 100% section loss to isolated anchor bolt nuts at base plates. There are scattered broken welds at the interface between the post and base plate. Isolated cracked welds at the South Railing

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REMARKS

have been repaired since the previous Routine Inspection (3/1/2023). See the following for specific locations and conditions.

North Railing

- First post from west: Tipped to the west and cracked weld along the base plate 2-1/2" at north and south.
- All railings anchor bolt nuts have light to moderate rust. Isolated anchor bolt nuts up to 100% section loss.

South Railing

- First post from west: Previously noted cracked weld at base plate repaired.
- **DEF S-A: Second post from west: Tipped to the east and cracked weld along the base plate full length at north and 3-1/4" at south.**
- Third post from west: Tipped to the east. Previously noted cracked weld at base plate repaired.
- **DEF S-A: Fourth post from west: Tipped to the east and cracked weld along the base plate full length at north and 2-1/2" at south (Photo 10).**
- All railings anchor bolt nuts have light to moderate rust. Isolated anchor bolt nuts up to 100% section loss (**Photo 11**).
- East end of railing: Easily moved by hand.

ITEM 59 - SUPERSTRUCTURE

Item 59.4 - Girders or Beams

The beams have scattered areas of light rust throughout and isolated locations of heavy rust. The beam ends are partially encased in the concrete backwalls (**Photos 12-13**). The visible portion of the beam ends have isolated locations of up to 5/16" section loss (0.13" remaining). The beam properties vary from 21" WF 59 beams specified in the design plans (**Sketch 4**). There are typically 1" diameter holes in each bottom flange over the abutments. There is an isolated location with six (6) 1" diameter holes in the web near the beam end. See the following for locations and conditions.

Beam 1

- Web and bottom flange at West Abutment: Up to 1/16" section loss.
- Web and bottom flange at East Abutment: Section loss (**Sketch 5**).

Beam 2

- Web, top flange and bottom flange at East Abutment: Section loss (**Sketch 6 and Photo 12**).

Beam 5

- Web and bottom flange at West Abutment: Section loss (**Sketch 7 and Photo 13**).

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REMARKS

- Web and bottom flange at East Abutment: Section loss (**Sketch 8**).

Item 59.9 - Bearing Devices

The bearings are not visible and typically embedded in the concrete backwalls (**Photos 14**). This differs from the available plans.

Item 59.14 - Paint / Coating

There is typically scattered freckled rust throughout the superstructure with isolated locations of failed paint and section loss to isolated beam ends (**Photos 8, 12 and 13**). All remaining paint is chalked.

ITEM 60 - SUBSTRUCTURE

Item 60.1 - Abutments

Item 60.1.a - Pedestals

The pedestals are partially embedded in the concrete backwalls (**Photo 14**). The visible portion of the pedestals are typically spalled up to full length x full width x full height. See the following for specific locations and conditions.

West Abutment

- Beam 1: Two (2) up to 3" long x 3" wide x 2" deep spalls.
- Beam 5: 12" long x up to 6" wide x full height spall.

East Abutment

- Beam 1: 12" long x up to 5" wide x up to full height spall.
- Beam 2: 7" long x up to 7" wide x up to full height spall.
- Beam 3: 12" long x full width of visible portion x full height spall (**Photo 14**).
- Beam 4: 12" long x up to 5" wide x up to 1-1/4" high spall.
- Beam 5: 12" long x full width of visible portion x full height spall.

Item 60.1.b - Bridge Seats

There is typically light accumulation of debris through the bridge seats at both abutments and moderate to heavy accumulation below the overhangs. There is an isolated area of scaling. See the following for specific locations and conditions.

- Below Beam 5 at East Abutment: 3'-0" long x 4" high x up to 6" deep area of scaling that extends onto the breastwall (**Photo 15**).

Item 60.1.d - Breastwalls

There are scattered areas of abrasion and an isolated area of scale. See the following for locations and conditions.

West Breastwall

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REMARKS

- At the water line: Full length x up to 1'-2" high x 1-1/2" deep area of abrasion, worst at southeast end (**Photo 16**).

East Breastwall

- Below Beam 5: Area of scale. See **Item 60.1.b - Bridge Seats** for additional comments.

Item 60.1.e - Wingwalls

There are isolated areas of scale at the top of the Northeast and Southeast Wingwalls. See the following for locations and conditions.

Northeast Wingwall

- At the top: 2'-0" long x 6" high x 3" deep area of scale.

Southeast Wingwall

- At the top adjacent to the East Abutment: 1'-0" long x 1'-0" high x up to 4" deep area of scale (**Photo 17**).
- At the top: Up to 13'-0" long x 5" high x up to 8" deep area of scale (**Photo 17**).

Item 60.1.h - Footings

The footings are hidden by design.

Item 60.1.j - Scour

There is an exposed steel sheet pile along the West Breastwall, that extends along both wingwalls full length x up to 2'-8" high at north end and 2'-1" high at south end (**Photo 16**).

SubStructure Scour Notes

See **Item 60.1.j - Scour** for additional comments

ITEM 61 - CHANNEL AND CHANNEL PROTECTION

Item 61.1 - Channel Scour

The channel flow is shifted towards the West Abutment which differs from the original plans. The plans show the flow distributed evenly below the bridge. There is a scour hole along the sheet pile in front of the West Abutment full length x 4'-0" wide x 3'-0" deep. See **Item 60.1.j - Scour** for additional comments.

Item 61.2 - Embankment Erosion

The Northwest Embankment has an area of erosion 2'-0" high x up to 1'-0" deep.

The Southwest Embankment, just downstream from the wingwall has an area of erosion with undercutting and exposed tree roots up to 4'-0" high x 3'-0" deep (**Photo 18**).

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REMARKS

TRAFFIC SAFETY

Item 36a - Bridge Railing

The bridge railings consist of two steel pipe rails, mounted through steel I-posts, bolted to the reinforced concrete rail bases. See **Item 58.8 - Railing** for additional comments.

Item 36b - Transitions

The east transitions consist of single concrete posts. The posts are tipped and severely spalled with no rail in place (**Photo 19**).

There are no west transitions in place (**Photos 3-4**).

Item 36c - Approach Guardrail

The east approach guardrails consist of single concrete posts. The posts are tipped and severely spalled with no rail in place (**Photo 19**).

There are no west approach guardrails in place (**Photos 3-4**).

Item 36d - Approach Guardrail Ends

There are no approach guardrail ends in place (**Photos 3-6**).

Sketch / Photo Log

- Sketch 1 : Location Map.
- Sketch 2 : Framing Plan.
- Sketch 3 : Channel Profile Measurements.
- Sketch 4 : Beam Cross Sketch.
- Sketch 5 : Beam 1 at East Abutment, North Face.
- Sketch 6 : Beam 2 at East Abutment, South Face.
- Sketch 7 : Beam 5 at West Abutment, North Face.
- Sketch 8 : Beam 5 at East Abutment, South Face.
- Photo 1 : North Elevation, looking south.
- Photo 2 : South Elevation, looking north.
- Photo 3 : Bridge from West Approach, looking east.
- Photo 4 : West Approach from bridge, looking west.
- Photo 5 : Bridge from East Approach, looking west.
- Photo 6 : East Approach from bridge, looking east.
- Photo 7 : Typical Wearing Surface, looking southeast.
- Photo 8 : Typical Underside, looking east. Scattered freckled rust with isolated locations of failed paint and section loss to isolated beams.
- Photo 9 : Underside of deck at East Abutment in Bay 3 with honeycombing and hairline cracks with efflorescence.
- Photo 10 : South Bridge Railing at 4th post from west with cracked weld along base plate.
- Photo 11 : South Bridge Railing at 2nd post from west with anchor bolt nut section loss.
- Photo 12 : Beam 2 South Face at East Abutment with section loss to web, top flange and bottom flange. Note: the beam end is embedded in the concrete backwall.
- Photo 13 : Beam 5 North Face at West Abutment with six (6) holes in the web and section loss to the web and bottom flange. Note: the beam end is embedded in the concrete backwall.
- Photo 14 : Beam 3 Pedestal at East Abutment with spall. Note: the bearing is embedded in the concrete backwall.
- Photo 15 : East Abutment bridge seat below Beam 5 with an area of scale that extends onto the breastwall.

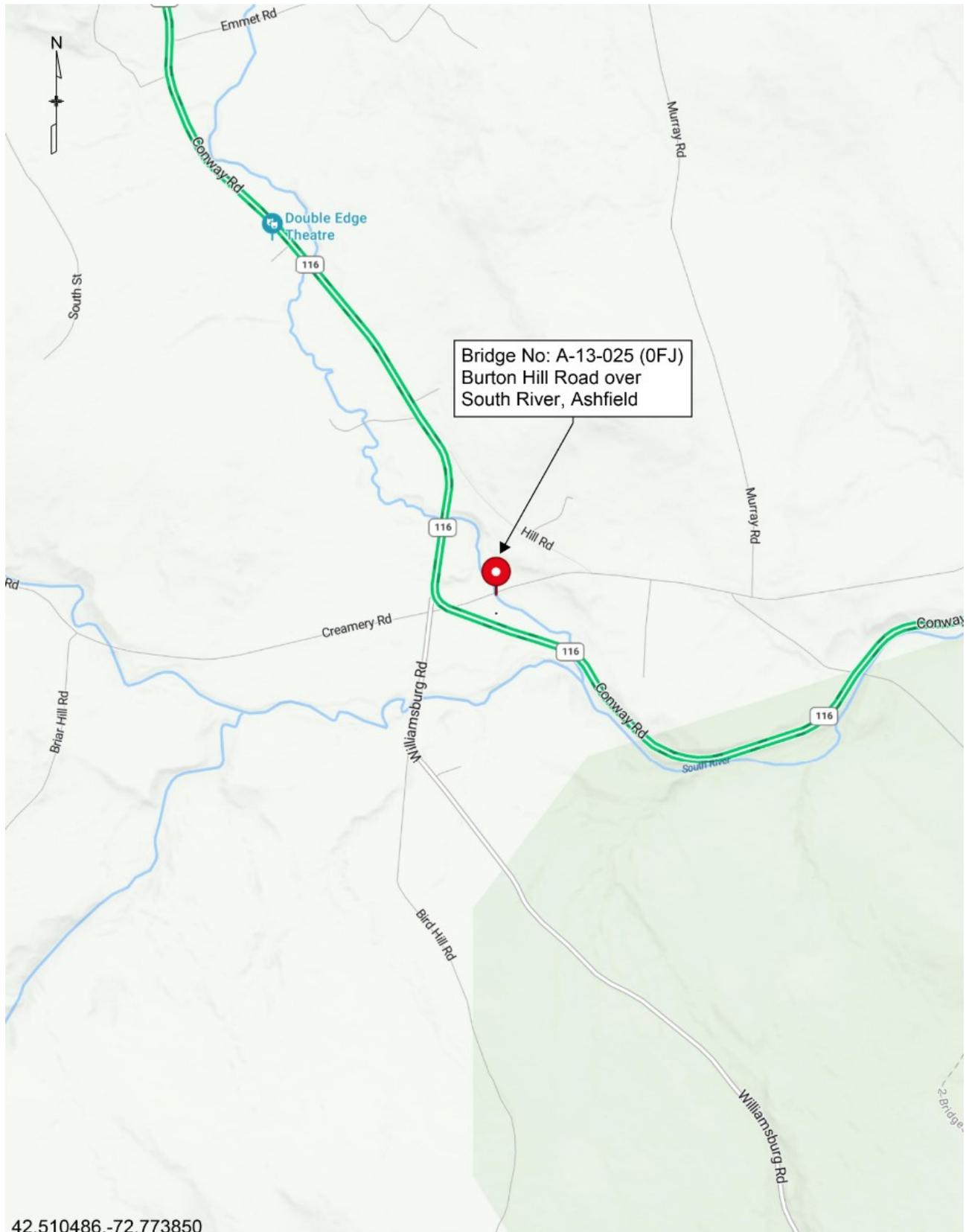
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REMARKS

- Photo 16 : Abrasion at the waterline along the West Breastwall. Exposed steel sheet pile along Southwest Wingwall and West Breastwall.
- Photo 17 : Scale along top of Southeast Wingwall.
- Photo 18 : Erosion with undercutting and exposed tree roots at Southwest Embankment.
- Photo 19 : Southeast Approach Guardrail posts are severely spalled with no rail in place.

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SKETCHES

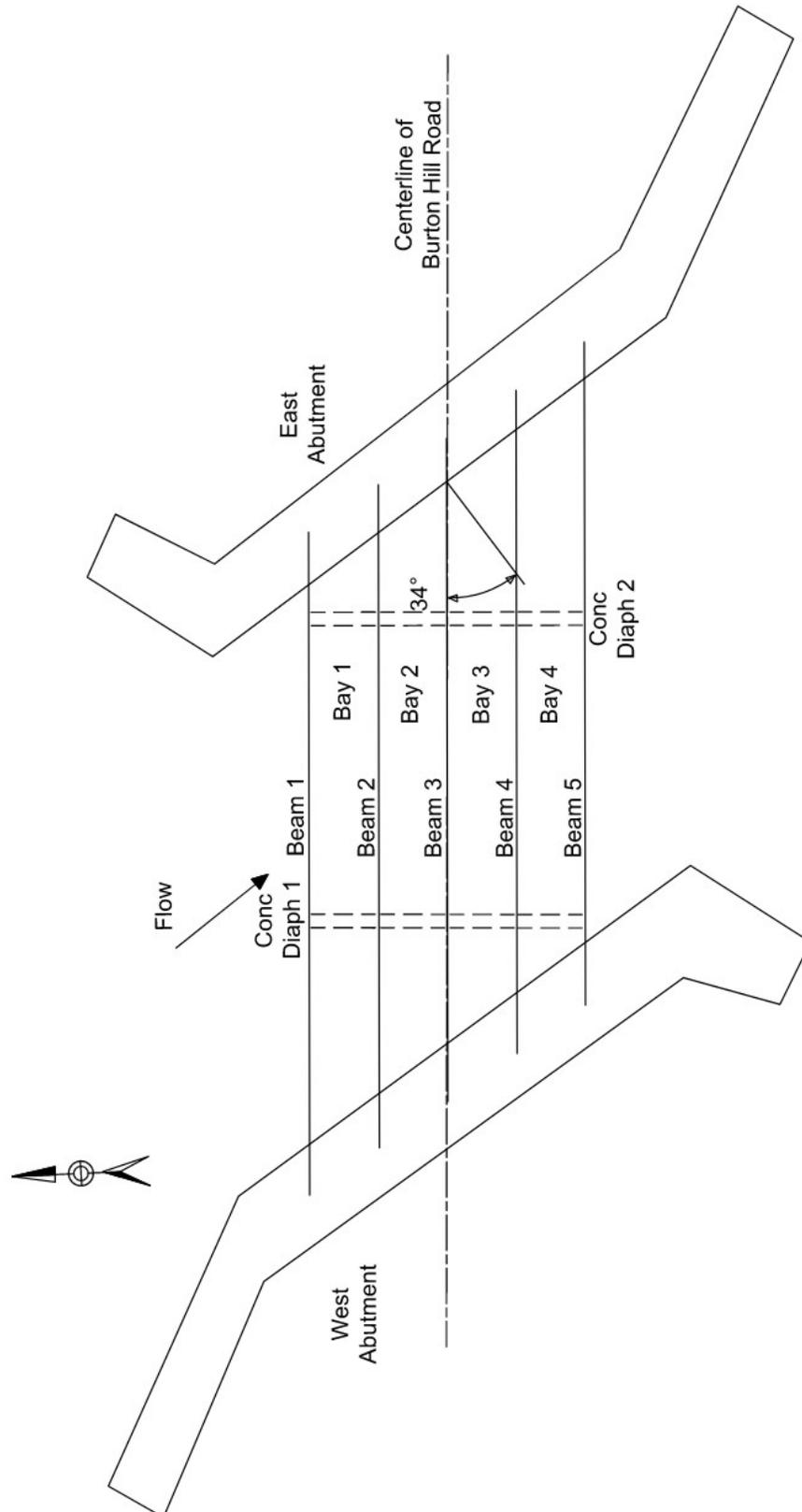


42.510486,-72.773850

Sketch 1: Location Map.

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SKETCHES



Sketch 2: Framing Plan.

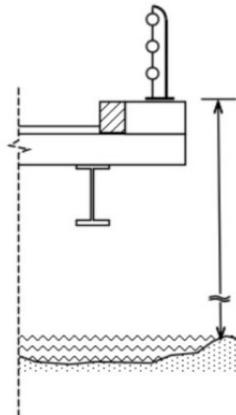
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SKETCHES

BIN 0FJ - Channel Profile Readings										
	North Fascia					South Fascia				
DATE	W. Abut	2nd post	3rd post	E. Abut	FB	W. Abut	2nd post	3rd post	E. Abut	FB
3/31/2025	10.3'	9.9'	8.5'	7.7'	8.6	9.1'	9.9'	8.2'	7.4'	8.3'

NOTES:

- * All readings in decimal feet.
- * All readings taken from streambed to top of railbase.
- * Posts of numbered West to East at both fascias.

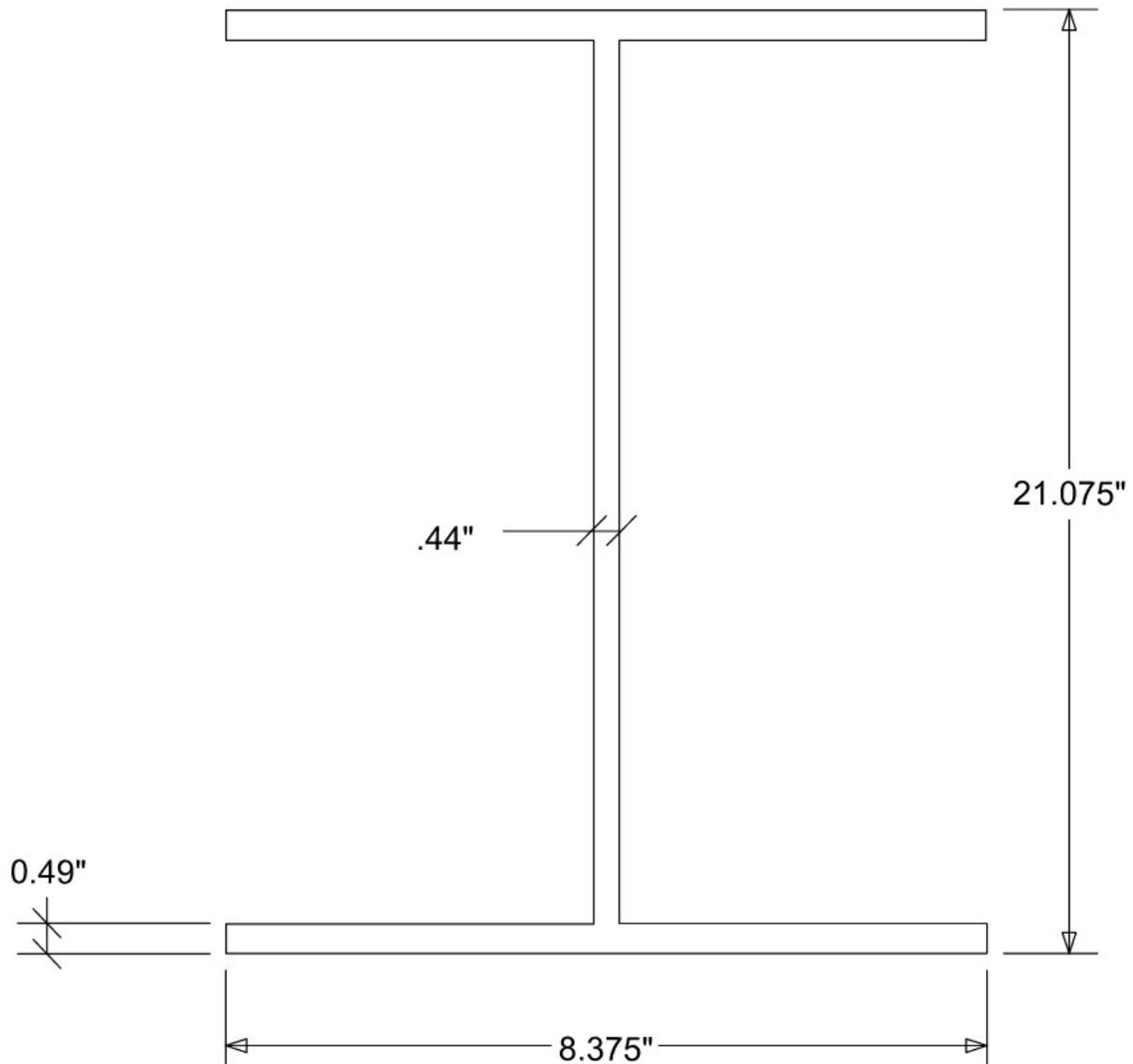


Sketch 3: Channel Profile Measurements.

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SKETCHES

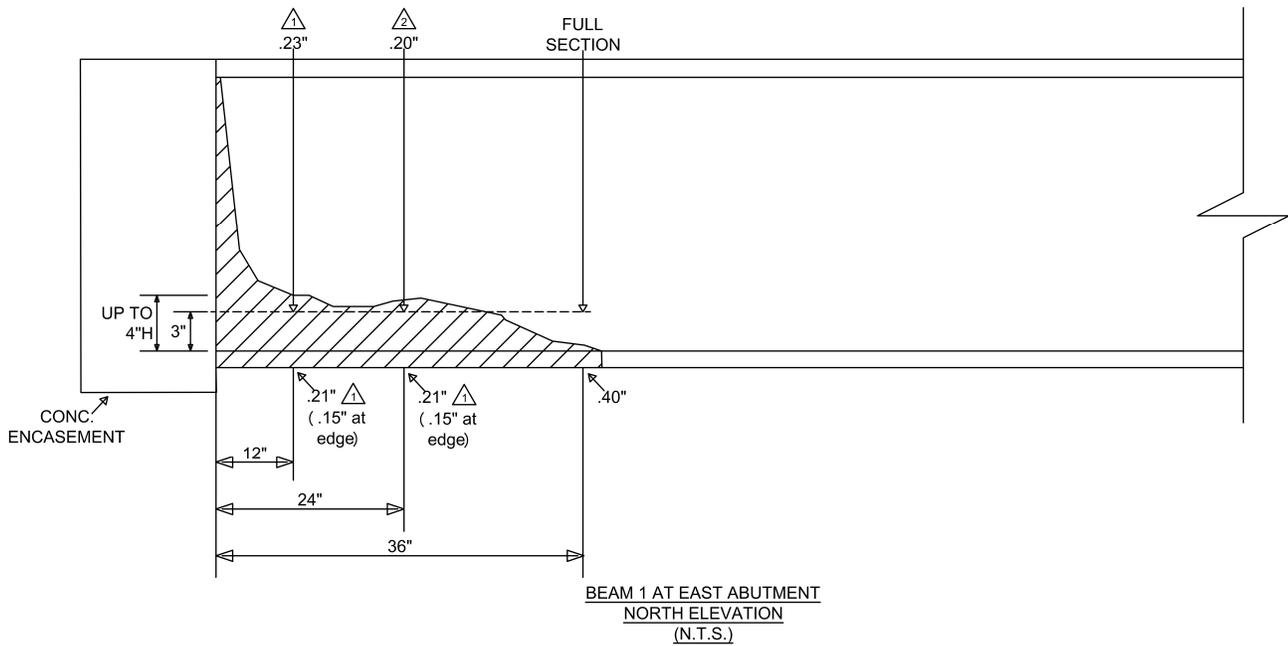
Field-Measured Original Section



Sketch 4: Beam Cross Sketch.

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SKETCHES



LEGEND:

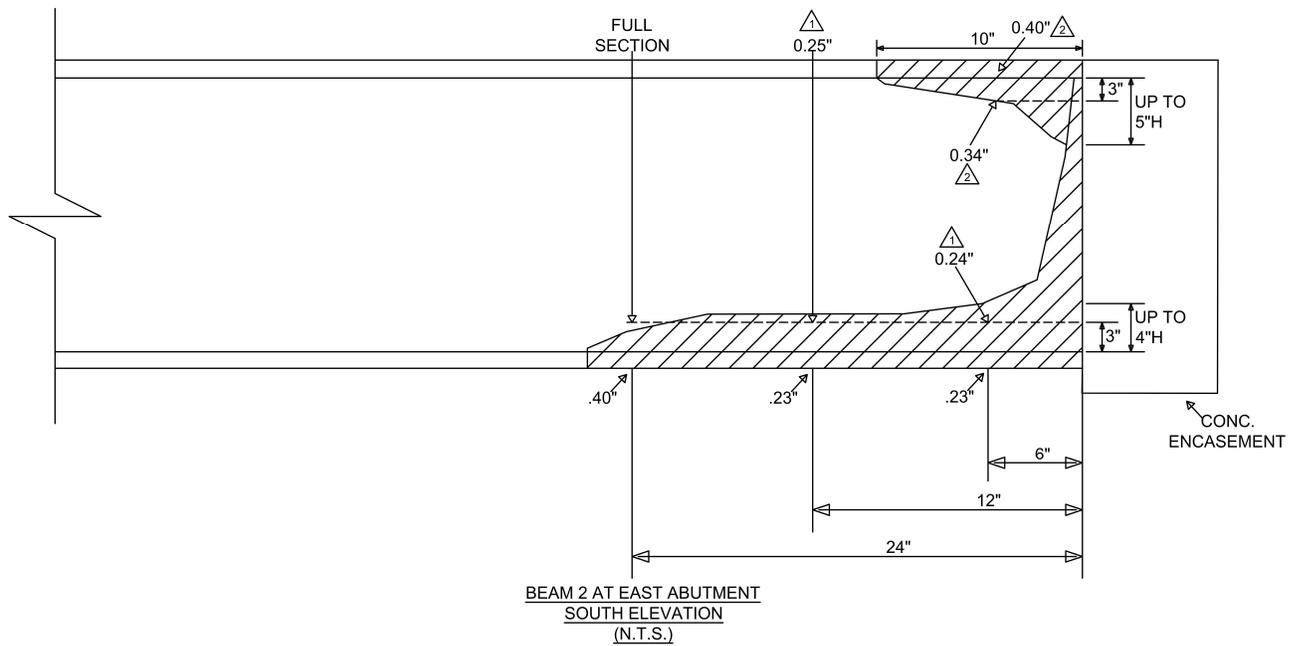
- 100% Section Loss
- Section Loss > 1/16"
- Indicates a change in condition from previous inspection
- Indicates a new condition not previously noted

AS-BUILT PER 1938 PLANS
 ORG. SECTION = 21" WF 59
 ORG. FLANGE = 0.49"
 ORG. WEB = 0.44"

Sketch 5: Beam 1 at East Abutment, North Face.

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SKETCHES



LEGEND:

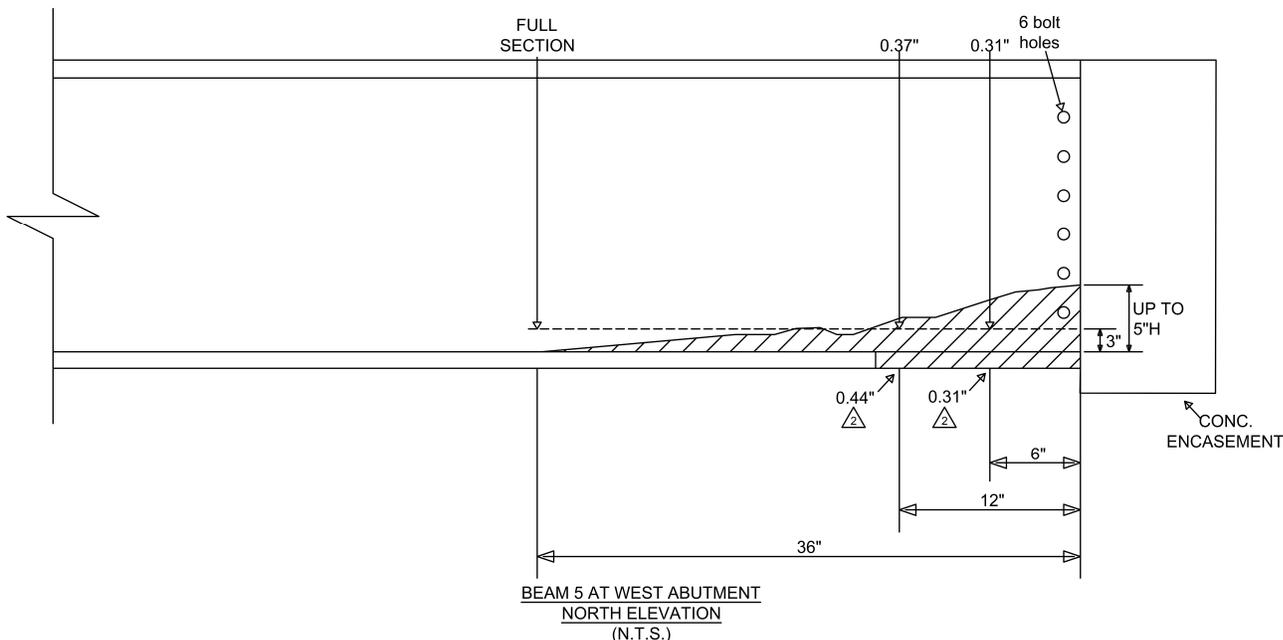
- 100% Section Loss
- ▨ Section Loss > $\frac{1}{16}$ "
- △ Indicates a change in condition from previous inspection
- △ Indicates a new condition not previously noted

AS-BUILT PER 1938 PLANS
ORG. SECTION = 21" WF 59
ORG. FLANGE = 0.49"
ORG. WEB = 0.44"

Sketch 6: Beam 2 at East Abutment, South Face.

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SKETCHES



BEAM 5 AT WEST ABUTMENT
NORTH ELEVATION
(N.T.S.)

LEGEND:

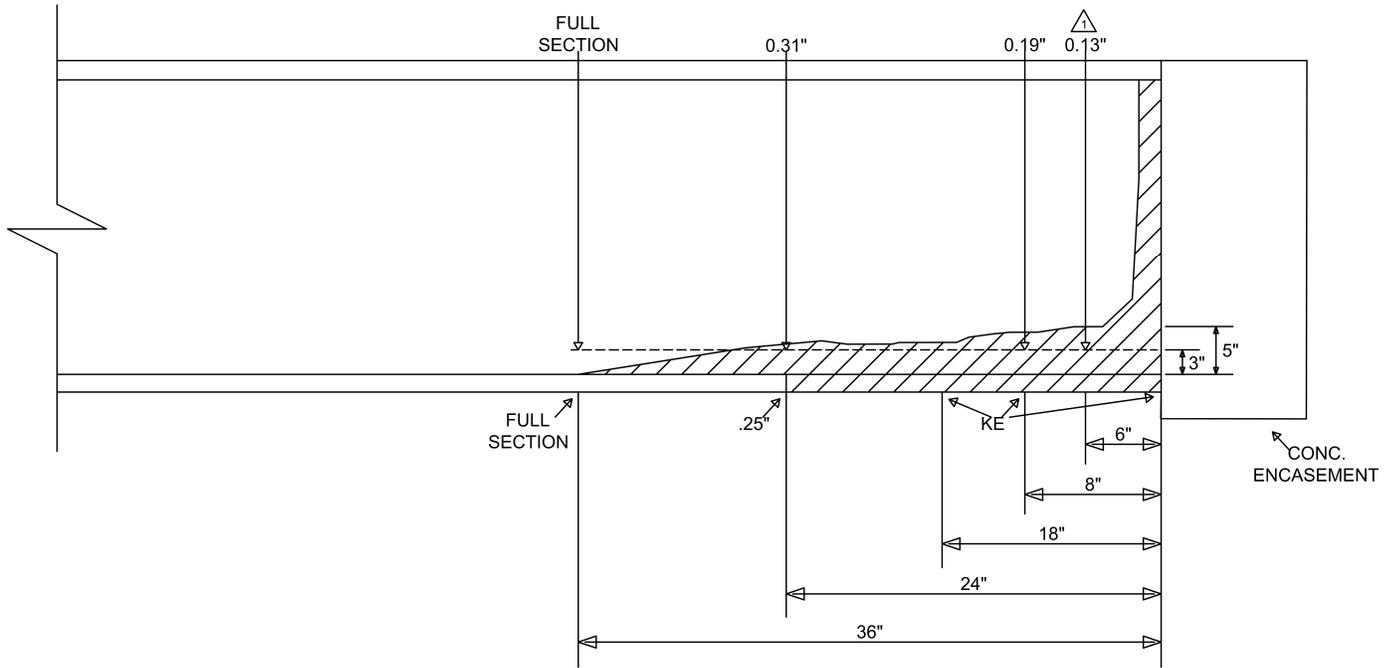
- 100% Section Loss
- ▨ Section Loss > 1/16"
- △ Indicates a change in condition from previous inspection
- △ Indicates a new condition not previously noted

AS-BUILT PER 1938 PLANS
ORG. SECTION = 21" WF 59
ORG. FLANGE = 0.49"
ORG. WEB = 0.44"

Sketch 7: Beam 5 at West Abutment, North Face.

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SKETCHES



**BEAM 5 AT EAST ABUTMENT
SOUTH ELEVATION
(N.T.S.)**

LEGEND:

- 100% Section Loss
- Section Loss > 1/16"
- △ Indicates a change in condition from previous inspection
- △ Indicates a new condition not previously noted

AS-BUILT PER 1938 PLANS
 ORG. SECTION = 21" WF 59
 ORG. FLANGE = 0.49"
 ORG. WEB = 0.44"

Sketch 8: Beam 5 at East Abutment, South Face.

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PHOTOS

Photo 1: North Elevation, looking south.



Photo 2: South Elevation, looking north.

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PHOTOS



Photo 3: Bridge from West Approach, looking east.



Photo 4: West Approach from bridge, looking west.

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PHOTOS

Photo 5: Bridge from East Approach, looking west.



Photo 6: East Approach from bridge, looking east.

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PHOTOS



Photo 7: Typical Wearing Surface, looking southeast.



Photo 8: Typical Underside, looking east. Scattered freckled rust with isolated locations of failed paint and section loss to isolated beams.

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PHOTOS

Photo 9: Underside of deck at East Abutment in Bay 3 with honeycombing and hairline cracks with efflorescence.



Photo 10: South Bridge Railing at 4th post from west with cracked weld along base plate.

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PHOTOS

Photo 11: South Bridge Railing at 2nd post from west with anchor bolt nut section loss.



Photo 12: Beam 2 South Face at East Abutment with section loss to web, top flange and bottom flange. Note: the beam end is embedded in the concrete backwall.

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PHOTOS

Photo 13: Beam 5 North Face at West Abutment with six (6) holes in the web and section loss to the web and bottom flange. Note: the beam end is embedded in the concrete backwall.



Photo 14: Beam 3 Pedestal at East Abutment with spall. Note: the bearing is embedded in the concrete backwall.

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PHOTOS

Photo 15: East Abutment bridge seat below Beam 5 with an area of scale that extends onto the breastwall.



Photo 16: Abrasion at the waterline along the West Breastwall. Exposed steel sheet pile along Southwest Wingwall and West Breastwall.

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PHOTOS

Photo 17: Scale along top of Southeast Wingwall.



Photo 18: Erosion with undercutting and exposed tree roots at Southwest Embankment.

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PHOTOS

Photo 19: Southeast Approach Guardrail posts are severely spalled with no rail in place.